

Results of the Pomeroy Village Public Survey

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Design of the Survey

The motivation for this public survey began several years ago. Concerns were raised in 2002 after the Massachusetts Highway Department presented the intersection of Route 116 and Pomeroy Lane, which culminated in the Town of Amherst requesting control of this section of Route 116 from the state.

The Design Review Board made observations and attempted to enumerate the design questions involved, and decided to instigate a public process to gather local preferences. A preliminary questionnaire for public distribution was drawn up, the content of which was revised and finalized in the spring of 2006 by planning intern Cana McCoy. Although immediate changes to the intersection are not scheduled, due to the Town's financial situation, it was decided that identifying the issues and working towards a plan would be productive. In addition to making sure the public is involved in the process as soon as possible, having plans and records of public opinions on hand might be helpful if the Town were to seek grants or other outside funding to move forward.

Distribution and Collection Procedure

The Pomeroy Village public survey used several distribution methods. Copies were mailed on May 10, 2006, to owners of all properties within the following bounds:

North	south of Crocker Farm School, including Kamins Circle, the Mount Holyoke Drive subdivision, and West Street frontage properties
East	west of Plum Brook, including Pomeroy Court and Pomeroy Lane frontage properties
South	north of Potwine Lane, including Orchard Valley
West	east of the Hadley line, including all frontage properties along West Pomeroy Lane

At the same time, copies were mailed to owners of businesses in the area, with contact information provided by the Amherst Chamber of Commerce. The total number of surveys mailed was 447. Blank surveys were placed along with two survey drop-off boxes in the neighborhood at the Amherst Athletic Club and the Hampshire Gymnastics School. Surveys were also placed at the General Cleaners laundromat, and provided for distribution to tenants of the Amherst Office Park and the Pomeroy Lane Cooperative. Finally, the survey was made available as a PDF file on the town website, and contact information was provided for any questions or comments.

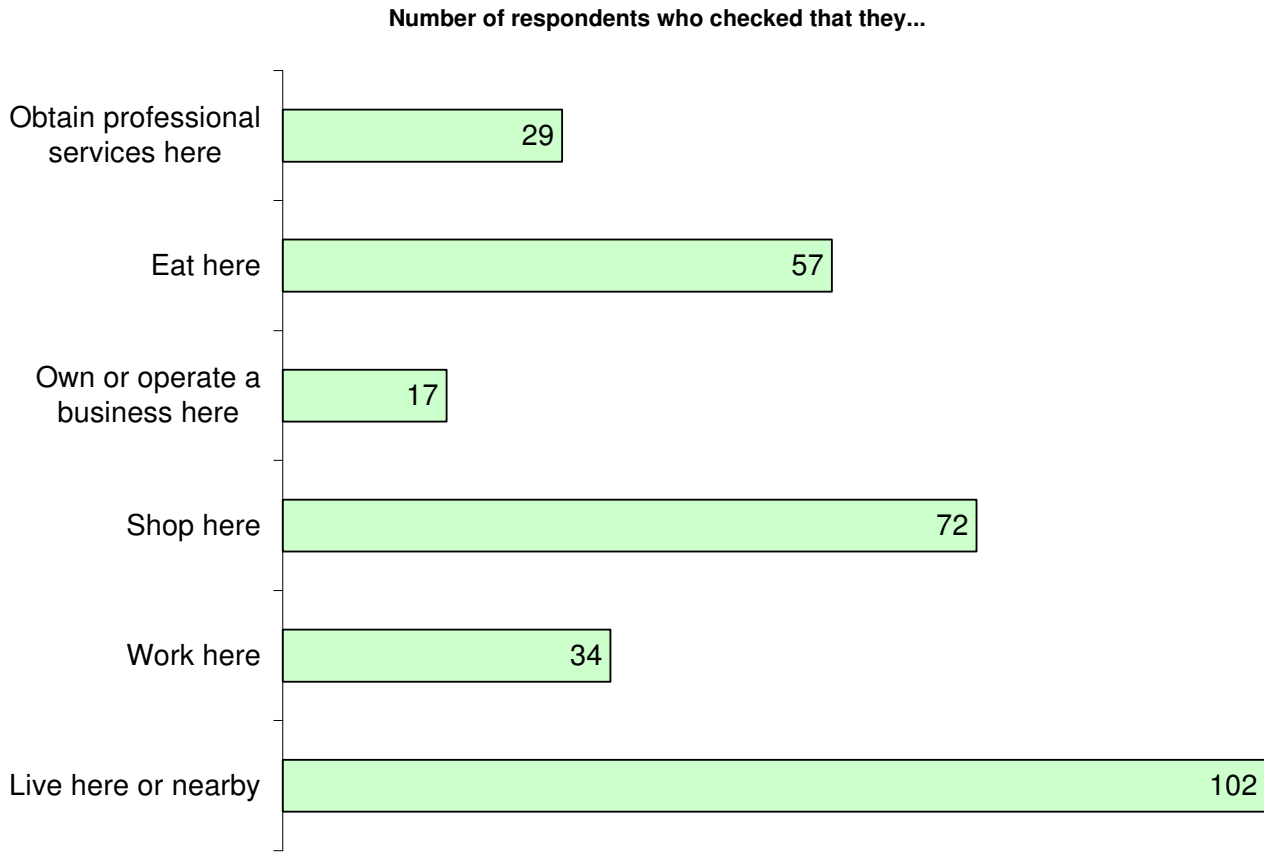
The majority of responses were received by mail, but a few dozen were collected from the neighborhood drop boxes or in person. As of June 20, 2006, the total number of responses was 128, which was considered by Town staff to be a strong showing of public interest. 77 of the respondents provided their names and contact information in order to be informed about public meetings and future development.

Makeup of Respondents

The survey asked respondents to tell us about themselves by indicating their connections to Pomeroy Village. While the largest percentage (80%) indicated that they were residents of the area, we also received completed surveys from employees of local businesses who live elsewhere in Amherst or in other towns (Figure 1). Receiving as many as 17 surveys from respondents indicating that they own or operate a business in the village center was also very welcome. The categories had significant overlap, with many neighborhood residents indicating that they also eat and shop in the village center.

Some respondents also wrote additional comments indicating that they pass through the village center on commutes, or that they own rental property nearby. Several respondents indicated that they were very long-time residents of the area. The written additional comments for this and all other survey questions are available as a complete list in the appendix at the end of this report.

Figure 1 - About the Respondents



Results by Topic

Roadways, Parking, and Bicycle Lanes

The survey questions for this section were:

Roadways

Add traffic calming features to slow traffic Narrow the road width
 Leave road widths the same Widen the roads

Bicycle lane width Full-sized (6 feet wide) Minimum (4 feet wide)

West Street
Pomeroy Lane
West Pomeroy Lane

On-street parking Yes No
If yes, *where* should on-street parking be located?
Where should it *not* be located?

Results

53% of respondents favor keeping the road widths the same or narrower (Figure 2). Since 74% of respondents also favor bicycle lanes on West Street (Figure 3), there is a conflict of desires that will require examination. If no widening of the road is desired primarily to prevent cars from passing through the village center very rapidly, this might be accomplished by carefully considering travel lane widths or traffic calming measures. 48% of respondents supported adding traffic calming to the roadways in the village center.

The results of this survey show strong opposition to the use of on-street parking in the Pomeroy Village area (Figure 4).

Figure 2 - Opinions on Roadways

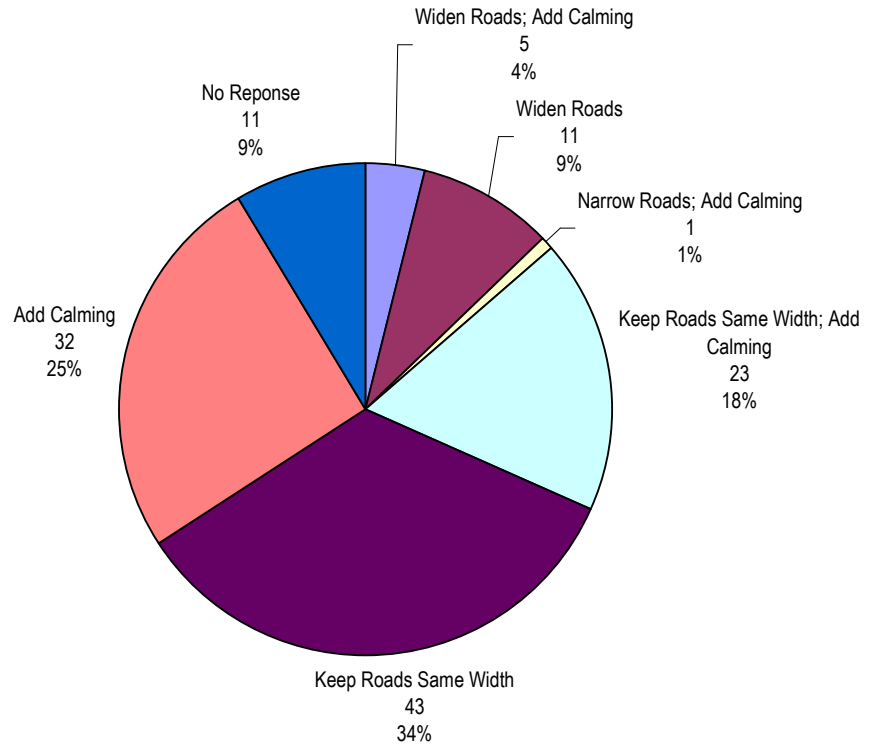


Figure 3 - Opinions on Bicycle Lanes

Bicycle lanes on West Street			Bicycle lanes on Pomeroy Lane			Bicycle lanes on West Pomeroy Lane		
Full-sized	63	49%	Full-sized	39	30%	Full-sized	37	29%
Minimum	32	25%	Minimum	48	38%	Minimum	48	38%
None	4	3%	None	4	3%	None	4	3%
No response	29	23%	No response	37	29%	No response	39	30%

Figure 4 - Opinions on On-street Parking

On-street Parking		
No	109	85%
Yes	4	3%
No Response	15	12%

Issues

The geometric design of roadways is an important topic in traffic engineering and planning. The capacity of a road is partly determined by the width of its lanes and shoulders. For many years, the standard reference on these matters has been the American Association of State Highway and Transportation Officials (AASHTO) publication known as the “Green Book”, which was last updated in 2001¹, and state highway department policies.

More recently, the context-sensitive design initiative begun by the Federal Highway Administration has urged greater flexibility in adjusting road widths and design to local circumstances.² The Congress for New Urbanism is currently collaborating with the Institute of Transportation Engineers to produce a guide for highly walkable streets in busier (mostly urban) environments.³ Some of the ideas coming from these movements, such as narrower lane widths to naturally reduce driver speed, can potentially be applied in a village center situation.

No questions were asked on the survey about the possibility of turning lanes on West Street, but this issue is important and should be discussed at any subsequent public meetings.

The questions on the survey regarding bicycle lanes did not provide an option to select for “no bike lanes at all”, which four respondents chose to write-in. This should be kept in mind when looking at the results.

On-street parking has been cited as a potential traffic calming mechanism, encouraging reduced speeds and driver alertness as well as providing a buffer between cars and pedestrians.⁴ It may be that its use as a form of traffic calming is little known or accepted. On-street parking may be considered inappropriate for this setting by many. It may also be that few people can visualize this former highway crossroads as a functioning village center.

Bus Stops

The survey questions for this section were:

Bus stops Keep existing stops Move existing stops Add new stops
 If bus stops need to be moved or new ones added, please specify where you want them to go:

Bus Shelters Yes No
 If yes, what should the shelters be like?

Results

A majority of respondents favor keeping the existing stops (Figure 5). Several of the respondents who suggested moving the current stops proposed very slight adjustments to the locations of the existing stops. Whether or not those who favor keeping the existing stops would approve of these slight changes in location should be explored.

Sixty-three percent of respondents favored placing bus shelters at the bus stops in this area (Figure 6), and there was extensive comment on what form these shelters should take. Most respondents appeared to favor simple shelters to provide weather protection, with many requesting shelters similar to those in use in downtown Amherst.

Figure 5 - Opinions on Bus Stops

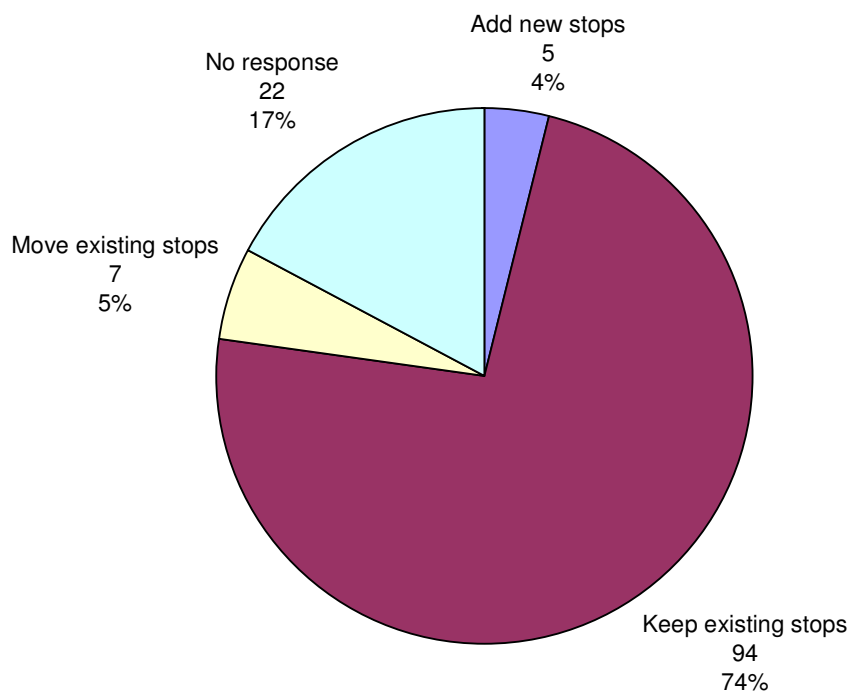


Figure 6 - Opinions on Bus Shelters

Should this area have bus shelters?		
Yes	80	63%
No	25	20%
No response	23	18%

Issues

The Pomeroy Village area is currently served by two bus lines, both of which are operated by UMass Transit. One of the five-college shuttle routes (38 – MT HOLYOKE-UMASS) provides north-south service along West Street, providing connections to UMass, Amherst College, Hampshire College, and Mount Holyoke College, as well as downtown Amherst and South Hadley. This bus is frequent by area standards, with headways of 30 minutes during peak periods, and service hours extending from early morning to the late night every day of the week. It is critical to note that this bus only operates when the colleges are in session. Winter and spring breaks receive reduced service, and the entire summer has no service whatsoever on this route. The second bus providing service to this village center (32 – WEST STREET) does run during the entire year, but has a very

small number of runs per day consisting entirely of weekday commuting periods. It is part of the system of outreach buses which are directly funded by the Town of Amherst.

When the five-college shuttle route is in operation, it covers the majority of the stops on the West Street outreach bus' route, with the exception of stops within the Orchard Valley subdivision and leading to and from the South Amherst Common. When the five-college shuttle route is not in operation, however, this village center has no transit service besides the outreach bus.

Currently, the three bus stops within the village center area do not have bus shelters. The bus stops on West Street both have paved pull-off areas for the bus, while the bus stop on the south side of Pomeroy Lane near the intersection (which services only the outreach bus) does not.

Crosswalks

The survey questions for this section were:

Crosswalks Just at the main intersection None Add others
 If other crosswalks are needed, please specify where:

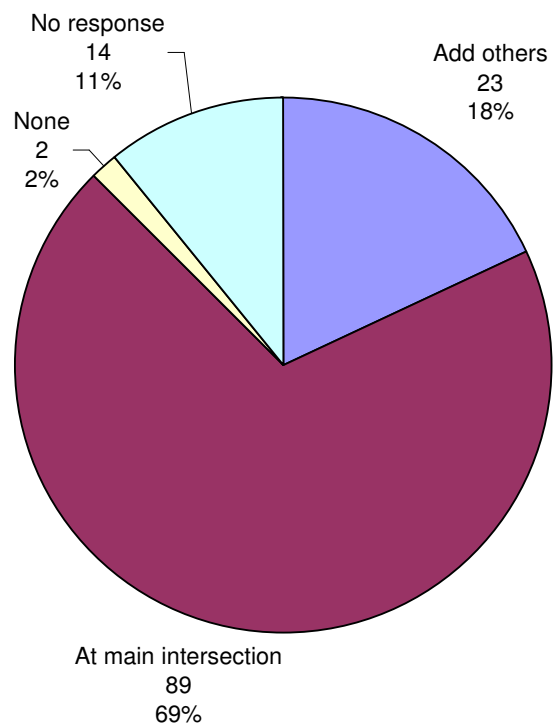
Results

87% of respondents favored adding crosswalks to the intersection of West Street, Pomeroy Lane, and West Pomeroy Lane, with 18% also favoring crosswalks added at other locations (Figure 7). The most frequent additional location mentioned was north of the intersection on West Street, approximately between the Dancer Computer building and the driveway between the Amherst Office Park and the South Towne Commons Slobody commercial building.

Issues

Adding a crosswalk to go along with the new traffic signal is a natural next step, but adding crosswalks in other locations is an action that will require careful consideration. Due to the way in which the ground falls north and south of the intersection, sight distances along West Street are somewhat limited when entering the village center. In addition, whether or not to provide marked crosswalks at locations that are not also controlled by a traffic signal or stop sign is an important question which has led to a great deal of research.⁵

Figure 7 - Opinions on Crosswalks



Sidewalks and Sidewalk Furniture

The survey questions for this section were:

Are new or extended sidewalks needed in the village center? Yes No

If Yes, where should they go? (You may also draw them on the included map)

What pavement material(s) would you prefer for sidewalks?

- Asphalt (black top) Concrete Pavers (brick, stone, pre-cast concrete)
 Other (specify): _____ It should vary

How wide should the sidewalks be?

- Minimum (5 feet) Medium (6 feet) Wide (8 feet) It should vary

What kinds of sidewalk ‘furniture’ and amenities should Pomeroy Village have?

- Benches/seating walls Newspaper Boxes Public Telephones
 Bicycle parking (loops) Trash/recycling containers Wireless internet
 Protective posts Drinking fountains Public art
 Others (specify)

Results

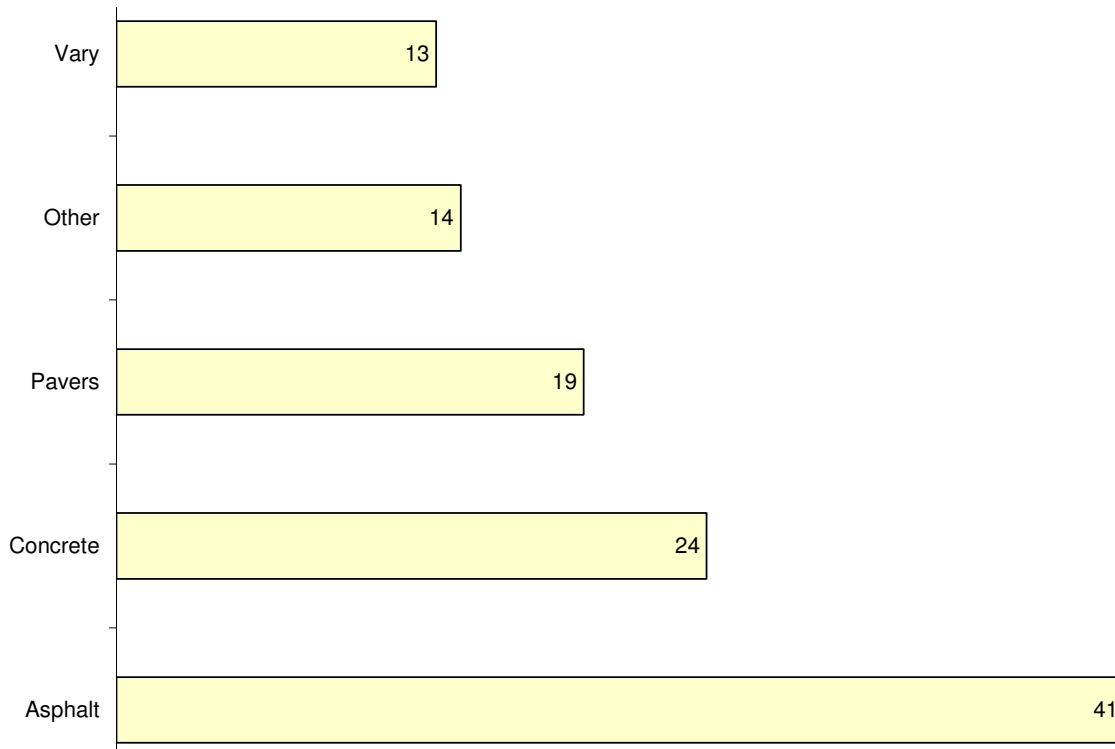
Figure 8

Are new sidewalks needed?		
No	35	27%
Yes	68	53%
No Response	25	20%

53% of respondents agreed that new or extended sidewalks are needed in the village center (Figure 8). In the extensive comments that were written in answer to this question, many respondents expressed concern about the financial strain of adding sidewalks in light of the Town’s current budget situation. Other respondents worried that additional paving would result in the removal of trees.

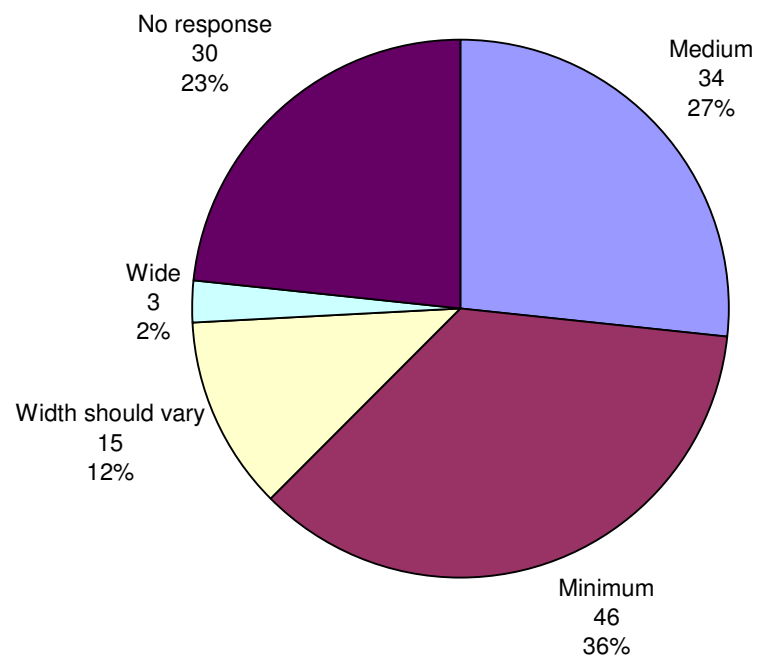
When suggesting locations for new sidewalks, several ideas were repeated quite frequently. A complete sidewalk is desired along Pomeroy Lane connecting the Pomeroy Village center with the South Amherst Common. A sidewalk is desired along West Pomeroy Lane between the main intersection and either Farmington Road or the Hadley border. The West Street sidewalks are reported to be in poor condition in some spots, and many respondents would like sidewalks along both sides of the road, in front of businesses such as the Hess station or the restaurants and shops on the southwest corner (Sibies, etc), or leading to all bus stops and potential crosswalks.

Figure 9 - Number of respondents who indicated support for each type of sidewalk material



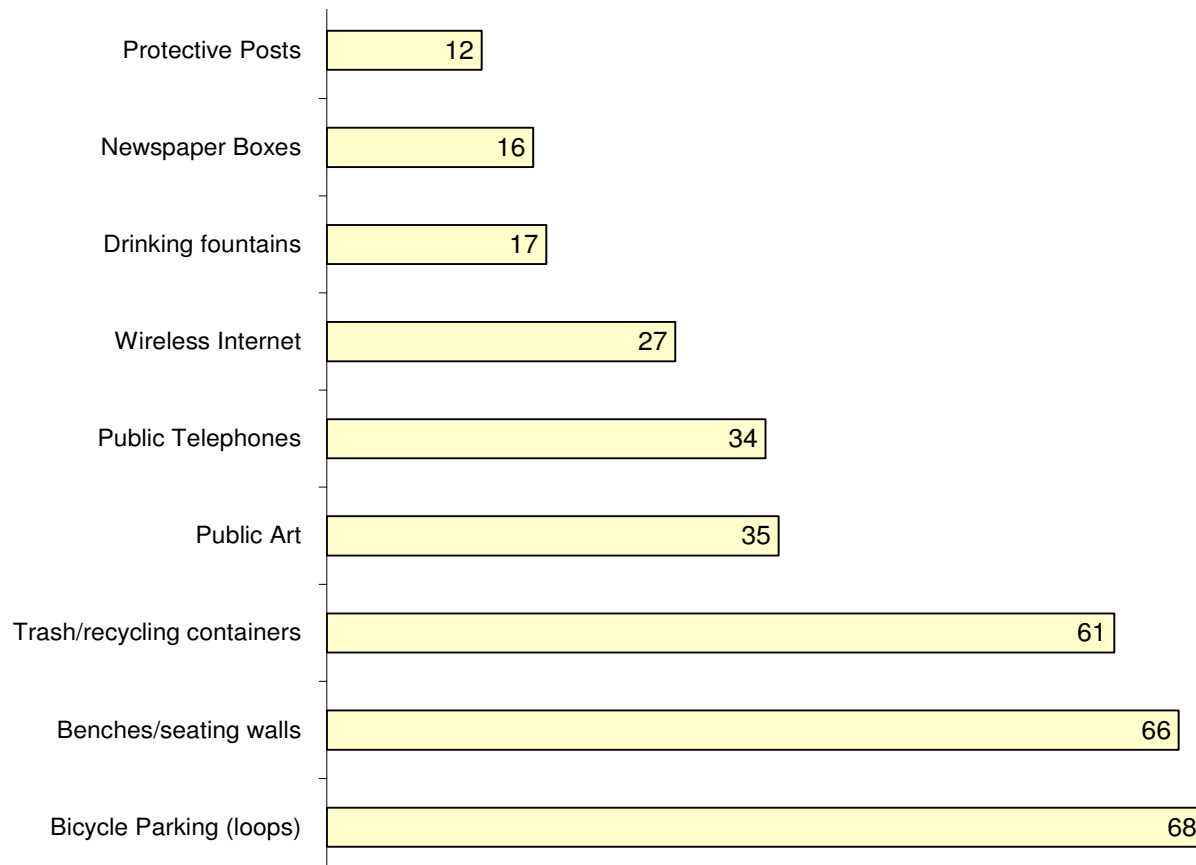
The survey also asked for opinions regarding sidewalk materials and width. Many respondents indicated approval of more than one potential sidewalk material (Figure 9), and the majority of respondents favored minimum-width sidewalks (Figure 10). In many cases, concern for cost was raised as an issue, with several respondents replying that whichever material is cheapest should be used. A couple of respondents pointed out that smooth sidewalk surfaces are important for people using wheelchairs.

Figure 10 - Opinions on sidewalk widths



Of the different types of sidewalk furniture suggested, the items checked most often were bicycle parking loops, benches or seating walls, and trash/recycling containers (Figure 11). In many cases, respondents indicated that the benches should be at bus stops, or that the bicycle parking should be in front of businesses. Several respondents indicated a strong concern for cost or for the possibility of vandalism. Several questioned how the amenities would be maintained in good condition.

Figure 11 - Positive Responses for Each Type of Sidewalk Furniture



Issues

Sidewalks are the backbone of the pedestrian experience in a neighborhood, and are important for the safety and mobility of everyone. The locations where sidewalks are provided are critical to shaping the paths where people will go, but they are often constrained by many factors.

In the vicinity of Pomeroy Village, several of the most desired sidewalk segments would need to overcome serious difficulties in order to be built. On Pomeroy Lane, a narrow bridge crosses the Plum Brook, and there is no room for sidewalks on the current structure. Since the sidewalk connection along Pomeroy Lane is such a frequently mentioned public concern, possible solutions should be examined when the bridge is next repaired or reconstructed. On West Pomeroy Lane, recent surveys have shown that the space of the public way along the north side of the road is much narrower than previously thought, but respondents have pointed out that the south side of the street has mailboxes and many large trees.

Along West Street, several respondents indicated that reaching local business would be easier if the sidewalks reached them. In many cases, the parking for these businesses is located in front of the buildings. Both these parking areas and the landscaped buffers that sometimes shield them can be problematic for pedestrians. Possible ways of connecting across these barriers to the sidewalks should be examined.

Sidewalk materials can set the tone for an area and give it a character distinctive from its surroundings, but issues of cost and accessibility should be considered. One respondent raised the important question of whether certain sidewalk materials are compatible with wheelchair use. Some research on this topic has concluded that certain types and arrangements of pavers compare favorably to standard poured concrete⁶, and this research should be examined if pavers are considered. Long-term maintenance costs of any given material should be considered alongside its initial cost when judging value.

The items being considered for sidewalk furniture are sometimes provided by private businesses on their own property, often as a condition of any required special permits. Coordination would be necessary if some amenities were also provided on the public way by the Town. "Adoption" of various amenities by local businesses might be utilized to both cover maintenance costs and provide greater exposure for local enterprises.

Informational Signs and Lighting

The survey questions for this section were:

Should there be signs 'announcing' entry into the village center at its edges?

Yes No If Yes, where?

Should there be 'crossroads' signs pointing people toward other destinations (colleges, museums, other villages, etc.)?

Yes No If Yes, where?

Should there be pedestrian as well as street lighting? Yes No

If yes, what kinds and where? (specify)

Results

A majority of respondents did not favor signs announcing entry into the village center area, but a crossroads sign pointing at other areas of town were favored by a small margin (Figure 12). In many comments, respondents indicated that they do not think of the village center as a separate entity, and do not want it to be seen that way. A couple of respondents mentioned that such signs might, however, be a useful way of calming traffic.

Respondents in favor of the crossroads sign indicated that it should go at the main intersection, and suggested that it should point to destinations such as downtown, the South Amherst Common and Munson Library, Atkins Corner, and the various colleges.

Figure 12 - Opinions About Informational Signs

Signs announcing entry into Pomeroy Village			"Crossroads" Signs		
No	67	52%	No	52	41%
Yes	39	30%	Yes	60	47%
No Response	22	17%	No Response	16	13%

Pedestrian lighting was favored by a slight margin (Figure 13), with many respondents commenting that it should be used only if pedestrian paths are not close enough to the streets to share the street lighting. A couple of respondents cautioned against lights that are open to the sky above and cause light pollution. Several respondents mentioned safety as the important concern. Several others indicated worry about the costs.

Figure 13 - Should there be Pedestrian Lighting?

Pedestrian Lighting		
No	44	34%
Yes	60	47%
No Response	24	19%

Issues

Signage can help to create a sense of place in a neighborhood. In deciding whether or not to have “entry” signs to the village center, both the identity of the neighborhood itself must be considered along with the attractiveness and maintainability of any potential sign. Crossroads signs would be useful for wayfinding. In all cases, long-term plans need to be made for the maintenance of any signs installed.

The International Dark Sky Association (IDA) and the Illuminating Engineering Society (IESNA) are in the process of developing a model lighting ordinance along with design guidelines.⁷ In addition, many municipalities nationwide have started drawing up and adopting their own regulations for lighting, with the goal of minimizing light pollution. If the Town of Amherst begins to formulate such a bylaw, any lighting added to Pomeroy Village should be designed to comply with it.

Landscape Issues

The survey questions for this section were:

Does Pomeroy Village need a central green? Yes No
 If Yes, where? (specify location(s))

Should the village center have more shade/street trees? Yes No
 If Yes, what kinds?
 Tall shade trees Mid-sized shade trees Small, ornamental flowering trees
 Traditional/native species Exotic species
 Where should they go?

What kinds of plantings should the public areas of the village center have?
 Small parks Flower beds & bulb gardens No-care shrubs & groundcover
 Planter boxes Just grass Others (specify) _____

Where should plantings go? (specify)

Results

Figure 14 - Opinions on Landscape Issues

Does Pomeroy Village need a Central Green?			Should the village center have more shade/street trees?		
No	62	48%	No	28	22%
Yes	36	28%	Yes	68	53%
No Response	30	23%	No Response	32	25%

The larger number of respondents indicated that Pomeroy Village does not need a central green space (Figure 14). Many respondents commented that a green would be nice, but they could not imagine anywhere that it would fit. Some respondents indicated that the South Amherst Common is a sufficient green space for the area. Those respondents that suggested locations for a green mentioned the parking lot of the businesses at the southwest corner of the main intersection, the vacant lot on the north side of West Pomeroy Lane just outside the intersection, or the former agricultural land between 410 and 450 West Street.

A 53% majority of respondents did agree that the village center could use more shade and street trees (Figure 14). Mid-sized shade trees were cited most often as appropriate, and approval of native species was much higher than exotic species (Figure 15). Many respondents wrote that trees should be added wherever there is room, and particularly along sidewalks and near bus stops. A couple of respondents recommended the avoidance of invasive species, and cited some invasive species that are already in the area.

Figure 15 - Opinions on types of trees the village center should have

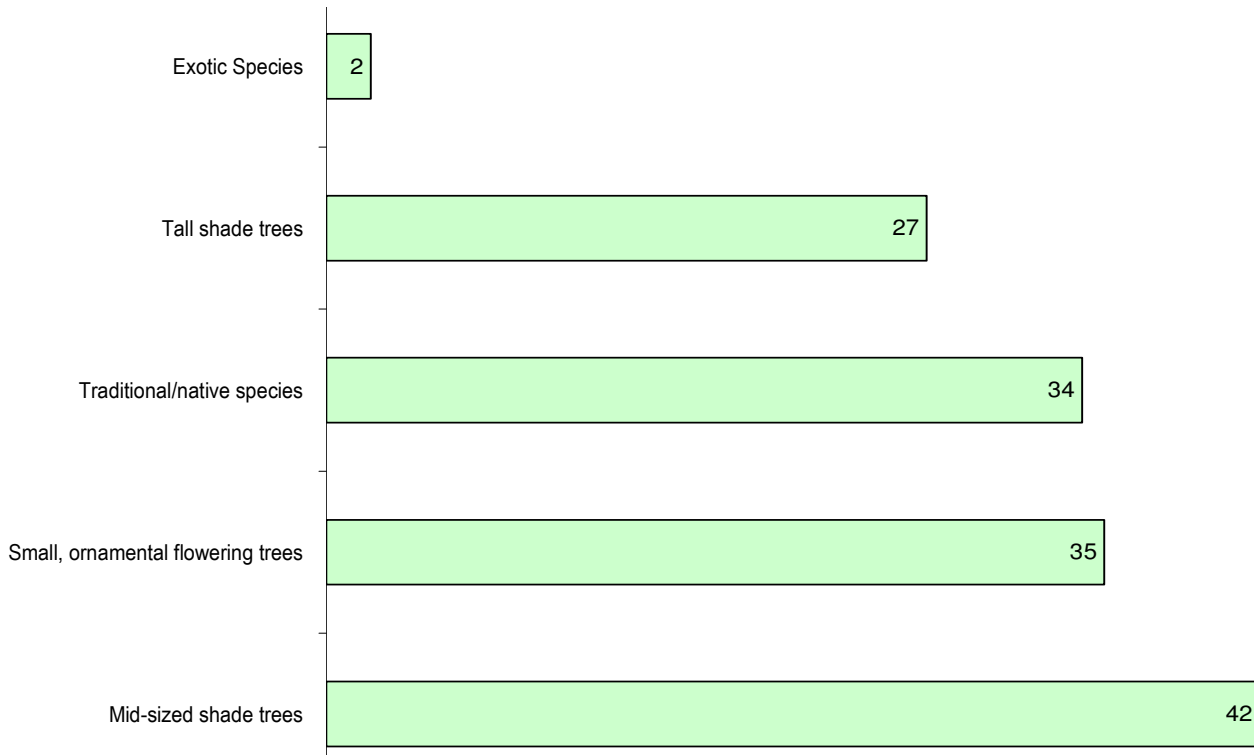
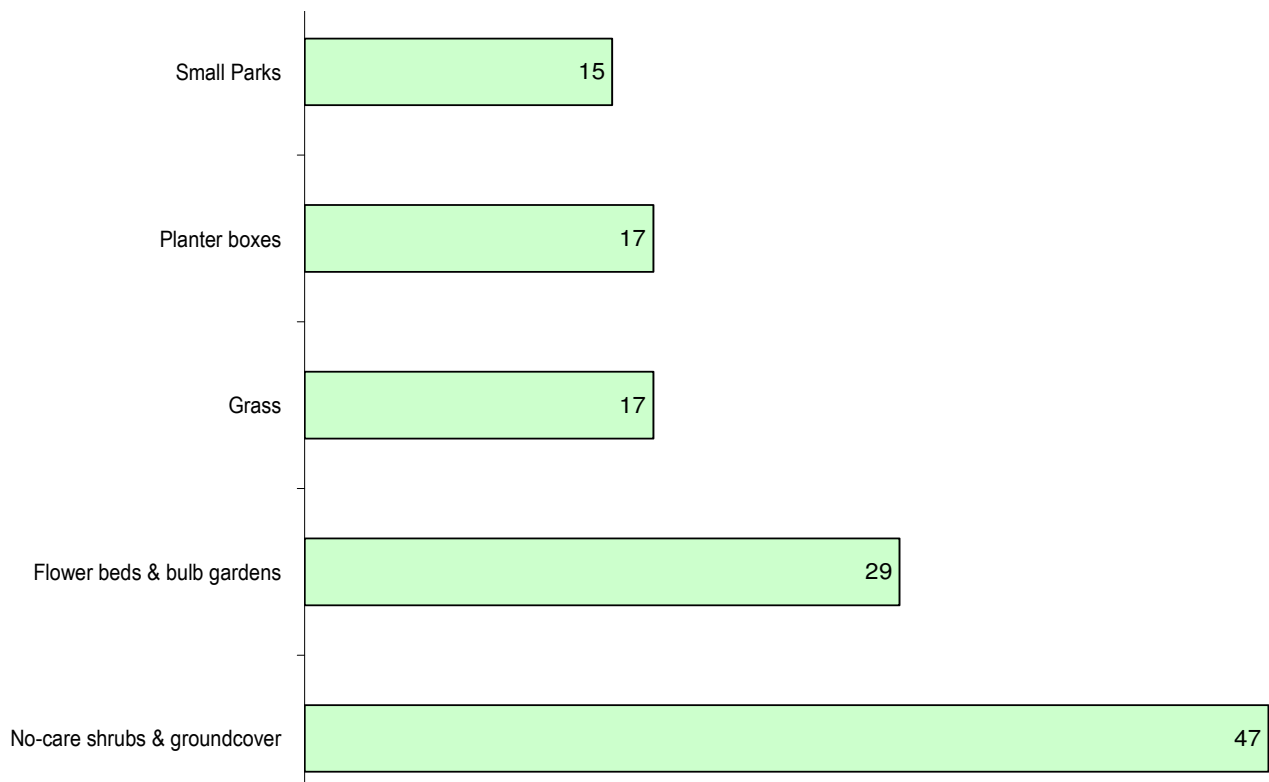


Figure 16 - Opinions on types of plantings the village center should have



Concerns about cost and maintenance were frequently cited in the written comments about plantings. Several respondents said that local businesses should be the ones to add landscaping to the area, especially since there is so much privately owned land. No-care shrubs and groundcover were the most frequently selected planting type (Figure 16).

Some interesting comments from respondents included a recommendation to create a community garden in the area, and a suggestion to have groups of local businesses compete to be declared the most attractive, tidiest village center of Amherst.

Issues

Landscaping is often examined by the Town during the special permit and site plan review process. As a result, many of the businesses in Amherst and in Pomeroy Village already feature attractive plantings and trees. This trend should be encouraged. The Town publishes a booklet of landscaping guidelines which is often given to developers. This information should be made even more widely available. This booklet includes information on species that are most appropriate for Amherst, as well of lists of species that are either not recommended or considered invasive by the state of Massachusetts.

Landscaping is often used to buffer parking, but since many businesses in Pomeroy Village currently have parking surrounding them, this landscaping can also serve as a barrier to pedestrians. When trees and plantings are planned for the village center, mobility for pedestrians on foot and in wheelchairs should be considered.

Additional Comments

At the end of the survey, space was provided for respondents to write in any additional comments they might have. A large number of respondents took the opportunity to comment extensively, and the complete text of these responses is included at the end of the appendix of written answers.

Many respondents mentioned improved safety as their primary goal for Pomeroy Village. Pedestrian safety in the form of crosswalks and sidewalks was emphasized, as well as the slowing of passing traffic. Several people indicated that they feel unsafe crossing Route 116 and that it “makes any pedestrian think twice”. One respondent worried that widening the road at all would cause more speeding, but several others expressed frustration that even the current speed limit is not well enforced.

Many respondents were extremely concerned about the cost of the potential improvements mentioned in the survey. Those involved in this planning process should make it clear that these are preliminary plans. It is known that money is not available to carry out these improvements in the near future. But we want to be prepared with plans when the resources do become available. In addition, it is possible that grants may be available for projects in the area. If such grants are located, successfully applying for them is made much easier if plans are available and the public is already involved. On the other hand, at least one respondent mentioned that evaluating streetscape improvements at such a preliminary phase without specifics is difficult.

Some respondents also addressed issues beyond the immediate questions of streetscape features. Several expressed a desire for more consistent “professional” signage on local businesses. Others hoped for ways to encourage new business that will cater to neighborhood needs, while some worried that increased commercial expansion will alter the character of the neighborhood. One respondent pointed out that commercial vacancy in the village center is quite high, while at the same time “we have a library so crowded for space that books are ranged in piles on the floor”.

Many respondents indicated that they don’t see the area as a village center, and some do not wish it to change. Several people objected to the name “Pomeroy Village”, either because they do not understand what “village” means in this context, or because they believe it makes the area sound like something other than part of South Amherst.

Hopefully, many of these issues will be raised and discussed as the public meeting where these results are presented. They will all become a part of the ongoing public planning process.

Appendix of Written Answers from Respondents

About the Respondents

- I drive through here a lot.
- Have lived here for 35 years
- We own rental property nearby
- I own property here
- I pass thru but I would pass thru more if I had a complete, safe bike route from town. Also I would walk there if less cars.
- My grandchildren go to gymnastics here
- Walk w/dog.
- Buy gasoline here plus newspapers and other non-essentials!!
- Chiefly use gas station, other shops on occasion
- I own a condo in 500 West St, I don't live there.
- [lived here] 30 yrs
- Lived in this neighborhood for 20 years
- I pass through
- Get gas/go to gym/use cleaners/take out pizza - because of current design, I almost always take my car to these things. Please make the village more enticing (and safe) to walk to!
- Own a home on Pomeroy Lane and rent office space in the Amherst Office Park
- There's not much shopping.
- I have to pass thru the proposed mess on my way to a real travel road, like 116 bypass, route 2, route 91 or turnpike.
- I helped a graduate student at UMass LARP with her master's thesis on this village center.
- Kids do gymnastics, teens buy snacks, we buy gas, newspapers, milk and occasional other groceries. Visit and use area businesses.

Roadway Widths and Traffic Calming

- (arrow pointing at bike lane question)
- (W. Pomeroy)
- [next to "Widen the roads"] No!
- [Widen] to accommodate bike lanes
- A wider road creates more problems. The present road slows drivers.
- Add left turn lane off West St
- But add bike lanes
- Do not change, do not add turn lanes.
- for right turns on red lights
- May require widening or narrowing to accomplish
- Need is for opposite - rev up not slow
- Please also consider lead-in roads e.g. Pomeroy, West Pomeroy
- Reason for changing?
- Slow traffic!! More monitoring
- Traffic speeds on W. Pomeroy, not enforcing protection of pedestrians
- Turning lanes
- West St turning lanes

Bicycle Lanes

- Does anyone use these?
- Don't put them on streets at all. If you have a lane for bikes the only safe place is an independent trail.
- No space
- Not enough traffic [on Pomeroy] for formal lanes
- Or not at all!! Preferable.
- Or sidewalk all the way to South Amherst Common & to Hadley
- Prefer good walk paths (sidewalks) to mixed-use or bike-only
- Prefer roads left as they are!
- Remove
- Use the efficient European model of bikers & pedestrians sharing a marked sidewalk/path.
- W. Pomeroy on south side only if it can be done without cutting down trees
- We are so far behind WA, OR, Californian towns (i.e. Davis, CA) in this area
- West St - Sidewalk was supposed to be bike walk. Would suggest combo on others
- West St has bicycle lanes, leave both Pomeroy & W Pomeroy lanes as is/replace if needed
- Wish to encourage biking
- Yes!

On-street Parking

- !
- [Not on] Pomeroy Lane, Southeast Street, West Street, West Pomeroy, Middle Street, Bay Road - Essentially no on street parking
- [Not on] Pomeroy or W. Pomeroy
- [Not on] West St, Pomeroy Lane, West Pomeroy Lane
- [Not on] West, West Pomeroy Lane, Pomeroy Lane
- [Not on] WestStreet, Pomeroy
- [Not] within various parking lots/internal spaces
- [Should be on] W. Pomeroy on one side, Pomeroy on south side. [Not on] West St
- Does not seem to be any practical place to locate - all development were required to have parking.
- Haven't thought about it yet
- It is a residential area and each house has its own parking. The few businesses already have their own sufficient parking.
- No need for it - there is plenty of parking available at all the local stores
- No no
- NO parking
- No preference
- Not anywhere on Pomeroy or West St
- Not on 116 - this is a very busy street. Parked cars on 116 would be a catastrophe
- Not on 116 or W Pomeroy or Pomeroy Lane
- Not on any busy street
- Not on Pomeroy Lane
- Not on Pomeroy Ln
- Not on residential streets.
- Not on Rt 116
- Not on street
- Not on the street!
- Not on the streets. There are lots for parking on all 4 corners of the intersection, which generally are not filled to capacity.
- Not on West St
- Not on West St
- Not on West St and Pomeroy
- Not on West Street
- Not wherever it would interfere with bike lanes
- Nowhere. There need to be lots.
- On-street parking is inappropriate for this area.
- On-street parking would help but needs to study as to where and how
- Should not be allowed! In downtown is garage that is empty! Take the empty bus on a tour! [Should not be] on the street where it takes away from the road/travel lane. If you want to park in road where is the cross town travel lane for commuters? Where is the bypass to get around this mess?
- There are off-street parking lots on all 4 corners of the intersection. The HESS lot is usually full and very busy, but the other 3 have plenty of spaces. For people to use those lots, the intersection needs to become pedestrian friendly - there are zero crosswalks across Rt 116. You need at least 2 on each side of Pomeroy, plus "traffic calming" signs or features!
- There is adequate parking for current businesses - new businesses should provide their own.
- There is enough off-street business parking
- There is plenty of parking in commercial areas.
- There is plenty of parking provided by the businesses.
- Too dangerous to bikers and small children getting out of cars. Distraction to others driving on 116.
- Unless the road is widened a lot
- West, Pomeroy, & W Pomeroy
- What will you do with all those cars?
- Whatever we have works for the people who live here and work here. Why fool around with a system that already works??

Where should moved or new bus stops be?

- (northbound) further north - by the mailbox, (southbound) further south, south of Pomeroy
- Add new stops every 1/4 mile - they are approximately at 1/2 mile intervals at present.
- At W. Pomeroy Lane and Rt 116
- Before the traffic light going south with pull off
- Eliminate UMass/Orchard Valley bus stops. Buses tear up the roads & cause cracks in house foundations. You should feel the vibrations!
- Entrance to Glendale/Orchard Valley on Rt 116
- Farther from the intersection

- I can tell you where I don't want them to go - directly in front of Courtyard Condominiums. We have more than enough noise/pollution from cars & buses right now.
- Just need better service midday, evenings, and weekends, of course
- Location for northbound buses on east side of West Street near gas station make accessible with shelter, buses should be able to fully pull off roadway.
- Make the stop at Hess station accessible (currently have to walk out into traffic to get to pull over). Likewise at Atkins Corner stops.
- Maybe add a bench & some plantings?
- More frequent schedules, please! [drawn on map - stop at corner of Coach Land and Pomeroy]
- Move southbound stop to south of intersection - won't interfere with life as much
- No opinion
- No preference
- No stops allowed keep the buses moving & not clogging up the commuter travel lanes.
- Not for southbound, but northbound should be moved a little farther north - right by Hess is way too congested & dangerous. [drawn on map - "maybe move northbound bus over here" indicating front of Dancer Computer building]
- Remove the ones there!
- Southbound stop closer to Amherst Office Park
- The Hess Station bus stop appears to be a good location, easily accessible to one of corners of this VC.
- The problem isn't too few stops, but that the buses don't run outside the academic year, which is a problem for low-income residents without cars.
- Traveling south on 116/stop should be slightly closer to north

Bus Shelters

- "wood" looking
- 3 sided, roof, inside bench
- 3-sided lean-tos
- A covered shelter with a bench to sit and be protected from bad weather.
- Anything that merely affords coverage & protection from the elements - & a bench/seating area
- At 5-College/West St only - should be same as downtown
- At least minimal rain shelter & seating should be provided at all bus stops if possible. On West Street waits are long.
- Atkins Corner; Hess station stops
- Attractive, not cheap aluminum or plexiglass
- Bench with roof over (next to Hess Station)
- Clear enclosures, benches would be nice
- Clear, attractive, smallish. No ads.
- Concrete and wood that gives the village center character and echoes nearby buildings
- Enclosed with benches
- for rain
- Glass roof
- Glass standing, like downtown
- Glass/plastic to cover and protect people
- I see people standing in the rain waiting for the bus.
- Just protect from rain and snow. Bike lockers
- Just to protect from rain
- Like the clear ones downtown
- Like the ones downtown
- Like the ones in the center of Amherst
- Minimal, no different than others at various places along the route, some seating (bench)
- Minimum: Head cover and & north wind break. Natural appearing. 4-5 person max
- Nice! Not metal - maybe wood
- No preference
- No preference
- One on east and west sides at the bus stops. Very simple cover with a bench inside - closed sides.
- Open
- Plexiglass with benches
- Quaint. Perhaps single-gabled.
- Roof and three walls
- Roof, at least one side with wall, bench
- Roof, glass sides
- Rooted with 3 sides closed, one open
- Same as downtown shelters
- Same as in downtown Amherst
- Shelters could resemble those in the center of town - with a bench to sit on and see-through walls
- Similar to the one at Amherst College
- Simple
- Simple - open
- Simple is good, but they could be fun if there is \$ for Amherst Cult'l Council to give grants to artists for design
- Simple, easily maintainable structures
- Simple, inexpensive, just to keep rain off

- Simple.
- Small but to accommodate riders in case of rain
- Small for 3-4 persons with bench & cover.
- Small shelters like those in the center of Amherst to protect from inclement weather - rain & snow
- Small wooden ones
- Small, covered
- Small/bench
- Something that can be maintained and kept clean - let's not put up unless we are prepared to maintain them
- Something w/"see thru" materials to be as invisible as possible
- Tasteful
- They become an eyesore
- They should resemble the ones in Amherst Center
- Three-sided with roof. Want to encourage people to come to area businesses even in bad weather.
- To stay dry in case of rain
- Water/snow proof; sturdy enough to break the wind in winter
- Wind & rain proof; no flyers

Where are crosswalks needed?

- safety important
- to encourage proper use
- at Mt. Holyoke Drive
- MAYBE. In reality - very few people cross at the light. Most jaywalk north/south intersection. Not sure crosswalk will be used enough.
- Kamins/Mt Holyoke
- On map: At main intersection (all four crossings), across West St in front of Courtyard Condos, and also at Dancer Computer driveway to Laundromat-driveway
- Amherst Office Park/General Cleaners to Chinese restaurant across 116
- Wherever bus stops are
- Closer to entrance to gym school as students cross street to go to convenience store
- [drawn on map]. Main intersection all four ways, and south entrance to Amherst Office Park
- One block north of Pomeroy/Rt 116 Intersection
- Defer to professionals to maximize pedestrian safety
- From office park to mailbox and bus stop
- North & South of business area to help employees & customers & bikes to cross conveniently
- Main intersection West St/Pomeroy Ln & West Pomeroy
- All 4 sides of existing intersection should have crosswalks
- Across from the Amherst Office park (see map) [note: map not included]
- Crosswalks plus "walk/don't walk" electrical signs. This intersection is currently only designed for cars not people.
- Glendale Rd, Longmeadow Dr, near USDA
- Crosswalk and a walk signal
- Others? There are none that I'm aware of. I asked the Select Board about 6 months ago to install them and they said the crosswalks were planned. It is dangerous crossing Rt. 116. See map for proposed sites. [drawn on map - crosswalks in all 4 directions at main intersection, and one between Dancer Computer and Office Park]
- Amherst Office Park
- Just North of intersection about 2-300'
- Amherst Office Park
- in vicinity of bus stop near Moan & Dove, Chinese restaurant
- Don't want another Amherst College!
- The crosswalks need to be handicapped accessible. After the installation of the light it is hard to cross the street if you are using a stroller, bike, etc.
- Now only one at intersection, perhaps 1 more needed at north side of light.
- At the traffic light
- From Amherst Office Park on West side of Route 116 to gas station on East side of 116.
- One also on 116 located about 2-300' north of 4 corner intersection. [also drawn on map - between Dancer building and 460 building]
- Pomeroy Lane/West St; Atkins/Hampshire College corner
- A crosswalk going from the Amherst Office Park to the east side of 116. [also drawn on map]
- More crosswalks from in between 463 and 479 (West St) across to services on other side [also drawn on map, in addition to main intersection crosswalks]
- Near the Montessori school & crossing on West Street

Where should new or extended sidewalks go?

- Both sides of West St. from gymnastics driveway South to Glendale Rd (on west side & to apts. on east side)
- The sidewalks are lumpy in the whole area
- Continue sidewalks from one end of Pomeroy Lane to the other, i.e. from Hess to South Amherst Common. Also fix crosswalk at southeast corner.
- Along West St in front of Hess, shopping complex and bank; the missing parts of Pomeroy (one side) should be filled in; also - wherever a lot of activity happens
- Certainly Pomeroy W & Pomeroy from golf course to library (SE Common) and entire (if not now done) West St into town center - without crossing and recrossing West St.
- [Material] Brick or stone, New England charm please
- Sidewalks should run along both sides of street
- Full length of Pomeroy Lane; East Side of West St to Crocker Farm school
- If no bike lanes, sidewalks needed for bikers & walkers
- Pomeroy Lane; W Pomeroy
- Extend sidewalk on Pomeroy Lane leading to Munson Library
- No one uses them and foot traffic is minimal. Like the sidewalks bike paths this is a waste of money.
- There should be sidewalks on both sides of the streets - both 116 and Pomeroy
- 1st priority - Extend sidewalk the length of Pomeroy Lane, currently there's a gap in the middle. 2nd priority - add sidewalk along W. Pomeroy Lane from West St to Farmington
- Enough already
- On the north side of W. Pomeroy because the mailboxes are almost all on the south side
- W. Pomeroy in conjunction with bikes
- Both sides of West St
- Extend sidewalk on Pomeroy Ln from Carriage to place where sidewalk begins east of Carriage.
- North side of W Pomeroy from West St to [edge of map], sidewalk and bike path
- I would like to see a sidewalk along the south side of West Pomeroy, from Farmington to West St
- (Asphalt is) best for in-line skates and other rolling equipment
- Can't say - I don't use them much and don't know where they are
- On West Pomeroy to Farmington
- Not sure - I never walk in the area, only drive
- Walks should also be on east side of Rt 116 near shops
- Amherst has, we have, a 4.7 million deficit. We don't need to beautify this commercial strip. That's just subsidizing businesses again at the taxpayers expenses.
- There should be sidewalks on West Pomeroy Lane.
- We are not a "village". We are part of Amherst. We are not interested in being someone's idea of "cute" or "New England" or "country".
- Perhaps on West Pomeroy on north side if more development on that side
- Along West Pomeroy Lane and East Pomeroy Lane - both sides
- [drawn on map] East side of 116, from intersection to crosswalk suggested
- What do you want to do build another \$800,000.00 sidewalk and raise taxes again!
- [drawn on map] south side of Pomeroy Lane
- They need a sidewalk on W. Pomeroy from town line to center so people can safely walk on that street. [also drawn on map, on south side of street]
- In a village center there should be sidewalks, that is the nature of a center
- On the south side of Pomeroy improved. Add sidewalk on north side of West Pomeroy all the way from Hadley Line to West St
- Your choice
- "See map, please" [But map was not included in envelope]
- From Pomeroy Lane south towards Glendale; on both sides of Pomeroy lane crossing 116
- Around the gas station, & center in general, so they are better visible, travelable, & plowable
- On the grass surface
- On the side of the courtyard condos; In front of Hess & So. Village square connected to crosswalks
- West St along W. Pomeroy to Farmington Road
- Need sidewalk extended ALL the way from West St to South Amherst Common - currently there is 0.3 mile w/no sidewalk, a lot of people walk that and the road is narrow and has sunset/sunrise problems for drivers. I walk to work here and had people NOT see me.
- Sidewalks should be improved on south side of intersection; also - the southbound sidewalk should be extended from Hampshire College that last 1/4 mile to Atkins Farms - that piece of 116 is dangerous to walk on.
- Please don't ruin the country feel - or what's left of it - by doing away with trees and lawn frontage to make room for sidewalks that few people would ever use. I don't want sidewalks! Not if it means losing frontage & bringing foot/road traffic closer to us.

- There are currently some sidewalks but they are unattractive & haphazard. For example, the main intersection has sidewalks on 2 of 4 sides. It should be made more pedestrian friendly for people in the neighborhood to WALK to the "center", as well as for people to get from one business (eg AAC) to another (eg Sibie's or Cleaners) [drawn on map - north side of W Pomeroy to vacant lot, south side to W Pomeroy Sibie's entrance. North and south side of Pomeroy to business entrances. West side of West St from intersection down to Sibie's plaza. East side of West street to botton of map.]
- From top of Pomeroy to join the lower
- east side of West St
- village center
- West Pomeroy Lane, fix sidewalks now obstructed on Pomeroy Lane at main intersection. On north side of Pomeroy Lane. [For material,] just safe, smooth sidewalks.
- A sidewalk on Pomeroy that connects to the So. Amherst Common. West Pomeroy is dangerous to walk on, a sidewalk would be nice up to the entrance for Orchard Valley.
- Sidewalks are needed in some areas. Currently people have to walk through parking lots to some locations. A planner needs to walk through the 4 shopping areas.
- On north side of Rte 116
- North side of West Pomeroy Lane. To Farmington (?).
- West Pomeroy Ln north side to Farmington
- W Pomeroy Lane
- All of Pomeroy Lane needs to have sidewalk.
- need sidewalks all the way up Pomeroy (including the area that currently has no sidewalk)
- Need sidewalk to be continuous from Rt 116 to South Amherst Common. Right now there is a 1/3 mile section w/o sidewalk and road is narrow and traffic is fast.
- The sidewalk on Pomeroy Lane going towards Middle Street needs to be extended from Carriage Lane until when the road begins. This will make walking possible for children to go to the Munson Library and occasionally for those who go to the So. Amherst H.S. This will also make the center fully accessible to those that live on Pomeroy Lane.
- North side Pomeroy Lane for the clustered housing close the center
- Pomeroy & W Pomeroy
- Probably
- Pomeroy Ln & West Pomeroy Ln
- West Pomeroy - from 116 to Farmington. Pomeroy - Extend existing sidewalk to fill in gap east of Coach Lane
- On West St, in front of the gas station so people can walk to the Chinese restaurant & the barroom. This is really the only sidewalk needed.
- I regularly ride my bike on these (seldom-used) sidewalks. They need some repair & maintenance but are otherwise adequate.
- Extend the sidewalk on Pomeroy Lane to Rt 116. Continue sidewalk on Pomeroy Lane so it is continuous from one end to the other end.
- The library & school budgets are being slashed and you're worrying about sidewalks! No.
- Do I understand that the Town Budget is being cut?
- Actually, yes to a bridge across 116.
- West St near Pomeroy intersection
- Both sides of West Street, just near the intersection. Extend sidewalk entire length of Pomeroy Lane, and further up (see map) north side of Pomeroy Lane. Replace & widen bridge/culverts over Plum Brook. [drawn on map - sidewalk on east side of West St from intersection to top of map. Sidewalk on Pomeroy north side to just before Coach Ln, continuing on south side "to end"]
- [drawn on map - sidewalk on north side of West Pomeroy]
- Ideally Pomeroy & West Pomeroy should have sidewalks going all the way to South Amherst Common & to Hadley. We should work with Hadley in expanding Moody Bridge & South Maple more accessible to bikers & walkers - it would make more bike path connection accessible and safer.
- Pomeroy Lane, please! Continuous sidewalk down Rt 116 to Atkins Corner. The corners of West St & Pomeroy Lane needs appropriate curb cuts!!! After light was installed, the curb cut was removed. Now I have to wheel my wheelchair out in the road to cross the street to Sibies/Andiamos. Sidewalks needed on South East St (whole length) to Bay Road & Bay Rd to Hulst Rd.
- Extending the Pomeroy Lane sidewalk all the way to the S. Amherst common would be nice.
- Both sides of the road
- Around the main intersection and businesses. Foot traffic is now dangerous.
- East side of West Street to #500 West Street condos from intersection. North side of Pomeroy Lane to Pomeroy Lane to Coop from intersection
- Between Amherst Office Park & Southtowne Commons near the mailboxes

Sidewalk Materials and Widths

- If you have them, the cheapest material.
- Trap rock [for material], even five feet seems too wide
- Medium width if shared
- No preference [for material]
- Whatever is easiest to maintain
- (Pavers) are a disaster downtown for people in wheelchairs
- Whatever is inexpensive
- Does not need to be fancy
- [Asphalt] is smooth enough for rollerblades
- What fits the design
- like the ones near Amherst Ctr
- Should be smooth, easy to maintain & cheap! [Width] should vary depending on whether or not it will accommodate bikers as well.
- Cheapest is fine
- Other: I know it's expensive but I do like what Amherst College has in front of their area on 116. The bumps on rt 9 are obnoxious, however.
- Whatever would work easiest for bikers & plowing & cheapest
- Wheelchair accessible/compatible
- Asphalt to match existing
- Material should be smooth and level!
- Recycled material like bike trail

Sidewalk Furniture

- Hess has a bicycle loop, but across the street probably does not
- No ugly extra stuff - keep it simple and quaint (not city characteristics)
- Benches at bus stops or within bus shelters. Do not see many bikes in the area.
- Not too much "furniture" unless there are major changes in the establishments.
- None - these would just be a target for vandals.
- LESS IS BETTER! Where is all the \$\$\$ coming from - more taxes?
- Nothing
- Bus shelters w/seating bench
- (Public Art) could be in form of bus stops, see above
- Have you completely lost your minds? Who is going to be using this stuff?
- No furniture - save the money
- None
- Pay or subsidize business that make restrooms available to the public (of course, they must be H.C.)
- Nice but simple plantings
- None
- A public toilet may be so unique it could become famous
- You may want to call us a town center, but the reality is we are not. Please - no sidewalk furniture!
- Safe walking conditions are more important.
- No newspaper boxes
- There should be seating at the bus stops. Bicycle parking should be encouraged. Internet for all would be wonderful.
- (crossed off recycling in trash/recycling containers)
- Public art would be nice
- Wireless tower for improving cell phone reception.
- Can the current mailbox by the computer business be moved to a more convenient location?
- NO public art.
- Green areas with benches - a little park where people would feel comfortable hanging out. Model after English villages/common areas.
- For bus stop: bench
- ATM machines, coinbox theaters & toilets & junk food machines. These should benefit the homeless - add bedding & toilet tissues. Who will shovel snow in winter?
- [Benches] by bus stop. [Bicycle parking] - maybe it would encourage people to use the PVTA more. [Trash bins] Who picks up/empties the trash bins?
- Emergency call boxes periodically on West St to Atkins corner
- Street lighting - more pedestrian friendly - like the new ones up town. Welcome to Pomeroy Village of South Amherst signs!
- Do not need! None, just bulldoze and pace over. Don't need anymore obstacles to the crosstown commuter lanes. It now takes far too long to get to a real travel road like Route 91 or Pike or Route 2, the kind of roads that go somewhere!
- Benches at bus stops. A small public park with land purchased from APR exclusion house lots. Bicycle parking at businesses.
- Benches near bus stop
- Allot space for above in plan. Await funding. Plan for progressive implementation.

Signs Announcing Entry into the Village Center

- It may be nice
- Wood carved signs; Get rid of sandwich signs please! They clutter!
- 1/2 mile before center
- North & South on Rt 116 of the intersection
- There are more signs now in area.
- At both brooks
- Not sure
- ?
- Any who decided this simple crossroads was "Pomeroy Village Center"?!? We are not & should not pretend to be a "village": We are part of "South Amherst".
- On West Street both ways as a safety measure
- From the south, just before the liquor store. From the north, just before the Goodwill bins
- North & South borders on 116
- ?
- At Amherst Office Park and Glendale Rd on 116
- Clever idea. It may have traffic calming features. [drawn on map: 116 SB before first Am. Office Park driveway, 116 NB before Courtyard. W Pomeroy EB before Sibies, Pomeroy WB at co-op entrance
- North & So ends of 116
- 100 yards ahead each way
- Maybe about 100 yards before each corner
- At edges
- You're kidding, right?
- On Rt 116 between Hampshire College and East Hadley Road
- See map. They would serve as traffic calming features. [drawn on map - at Muddy Brook on 116 and just north of Office Park on 116, on W Pomeroy in middle of vacant field, and on Pomeroy just after last co-op entrance.]
- At the entrance and possibly the exit
- Maybe
- Unsure at this early point
- By Crocker Farm, Kamel Hassan
- Never heard it called a village until this survey.
- I have lived here for 40 years and never heard it called a village.
- 1/2 mile on Rte 116 prior to the light on Pomeroy Lane
- The signs presently are the biggest eyesore! Going north on 116 on the right just beyond the Hess station. There is no uniformity. It looks honky tonk.
- If there is a need to identify this area separately (like Cushman Village) then perhaps signs would be nice.
- South Amherst on signs, NOT Pomeroy Village - motorists might think they were elsewhere than in southern part of Amherst.
- [drawn on map - Just after plum brook southbound on 116, at Glendale Rd northbound on 116]
- West Street (north end); South East Street
- Close to intersection
- More useless side of the road pollution that cost \$ to maintain & is another receptacle for graffiti.

Crossroads Signs

- I didn't know I lived there
- Toward Amherst center
- at intersection
- Waste of money. Most people using this area are local and know where they are going.
- At intersection where the traffic light is
- On 116 N & S
- Crossroad - Pomeroy/West St
- Leave it to a "designer"
- at intersection 116/Pomeroy
- Near bus stop?
- ?
- At the intersection of Pomeroy/West Pomeroy and Route 116
- Only on West St
- Hampshire College, Golf Course, S. Amherst Common, Munson Library
- at intersection
- approaching 116/Pomeroy Ln intersection from north and south
- Center
- Intersection
- At each corner
- at intersection
- Before intersection to alert oncoming traffic.

- At intersection of 116 & Pomeroy Lane
- No opinion
- At the 116/Pomeroy intersection
- At intersection
- Atkins! :)
- Eric Carle Museum, Hampshire, Amherst Colleges, UMass, Amherst Montessori School. Also to path near Plum Brook (a few hundred yards E on Pomeroy)
- Unsure at this early point
- On northwest corner
- West St/Pomeroy intersection @ stop light.
- At intersection of 116 & Pomeroy Lane
- Along Rte 116
- Near intersection of West St and Pomeroy Lane
- Intersection
- All four corners, for 116 going south maybe 2 - one by the prof. building driveway & another one right after the light at the corner of the Andiamo/Sibies parking lot.
- At or near intersection
- to S. Amherst Common - to Hampshire, Atkins
- See above
- Near intersection
- Anticipate/include Atkins Corner

Pedestrian Lighting

- Definitely at the main intersection - like that in center of town
- Near sidewalks & bus
- Periodic street lights for safety
- At intersection
- Along West St toward Crocker Farm
- There is enough lighting now.
- There should be at least one street light in the middle of each residential block.
- All developments are now lite, street lites should be provided for sidewalks
- centrally along the walkways
- W Pomeroy Lane
- Depends on whether ped. ways are near streets or not. Currently lighting of sidewalks is amply provided by street lighting.
- I work here and am not a night-time pedestrian - don't know if its needed
- Not sure
- There are at least a dozen street lamps out on Pomeroy. I reported this to DPW 5/8 or so.
- There should be pedestrian lighting on the last stretch of West Pomeroy Lane near the intersection.
- ?
- telephone pole lighting
- Lighting at bus stops
- At bus stops and near benches
- Considering the limited budget
- Not sure need to check out different lighting
- Tone down light from the gas station. That lighting completely changed the look and feel of this neighborhood, for the worse.
- At intersection to illuminate crosswalk
- Existing seems fine
- ? Don't understand what's meant
- Light standards in village center beginning at the edges
- What we have now is fine. Have there been complaints??
- Around the Pomeroy/116 intersection
- Again to keep pedestrians safe while walking on West St and along Pomeroy and West Pomeroy Lanes.
- Interior of complexes
- At crosswalks and bus stops
- X-walk lighting
- Street lights adequate - should consider types that downlight only to avoid lighting the night sky.
- At the intersection give pedestrians a turn to go across when a button is pressed.
- Along sidewalks
- Something similar in size to old-fashioned gas lamps; i.e., approx 10-12' tall
- West Pomeroy
- Well - what are the alternatives? You need to give more info here.
- If there is a park/bench area, it should be lighted.
- Just in center area - to conserve electricity.
- Side streets as well as main streets.
- In front of Amherst Fish Mkt parking lots. It's too dark at night (I lost 2 signs) outside.
- Pedestrian lighting within 100m of intersection, all 4 directions. Something tasteful, along sidewalks.
- If any lighting keep it low - not overhead.
- Near bus stops & crosswalks
- Intersection of 116 & Pomeroy

- Pedestrian lighting away from the center - solar ones to save energy? The center itself is lighted well with Hess there, I think.
- Use solar panels to light street lights to decrease electric costs.
- like uptown
- Costs \$ & no gain. Select board shut off lights because they did not have enough (?) tax dollars to maintain
- Street lighting should be enough if done correctly and maintained.
- Street lighting should not be open to the sky as it is in town center. All downcast not high, lower lights are more ped-friendly.
- Some street lights near bus areas and in parking lots
- Traffic lights on 116: green or yellow flashing "eye" for crossing traffic from midnight to 6am.

Central Green

- I don't know where it could be placed
- On the west side of West St - both N & S of the intersection. It's ugly right now.
- Nice flowering trees and gardens
- But where?
- the plaza on the SW corner of W Pomeroy and West
- Nice idea, but where?
- Where did this idea come from? Ridiculous.
- No space
- Completely unnecessary - we already have a wonderful S. Amherst Common
- Not sure if needed but would be nice if room allows
- It needs something central other than a gas station.
- Somewhere near the center - perhaps just behind the shops or the west side of 116
- Even though practically ever other center/village in town has one, I believe it would appear to be "contrived" if added. Had it occurred naturally over time it would be fine, but it didn't.
- It would be lovely, but not sure where it could go
- A low wall (like the one at College & So East St) saying Pomeroy Village - take down present stores sign (it's leaning anyway) (to go in SW corner)
- Not sure
- Northwest corner area of intersection
- We already have a green - across from our library.
- Nice idea but with present development no space seems available
- Impossible due to existing space - possibly community park on undeveloped land behind Valley Transporter
- How can you make a crossing have a central green? Those are in this case incompatible
- Where the parking lot of the little offices (S-E corner)
- Your choice
- West Pomeroy Lane [drawn on map: a square around vacant lot behind Southtowne Commons, labeled "new park/center"]
- Across from Amherst office park - that huge field!
- We have one. The South Amherst common.
- That would be nice, of course
- 3 acre parcel in front of Hampshire Gymnastics (i.e. south of 116) and east of Taylor Davis [also indicated on map]
- Not sure where it would go
- On west side of West St
- It would be nice but where to put it is a problem
- Not if you leave the area alone! We have plenty of trees & lawn right now.
- I don't know what this is. If you mean green open space, sure - but where could you put it? This is a major road. Make it a pleasant place - but on the other hand, a park or playground would not be best use of space on 116. Maybe on Pomeroy. [drawn on map - vacant parcel on W Pomeroy marked "Park?"]
- There is no room for a central green.
- The businesses are landscaping their properties nicely. I am not sure where there is any land for a central green.
- A small center green would be very nice but unsure where it would go. The area is too much like a strip currently.
- Across from Amherst Office Park or next to
- How about a village park on north side of W. Pomeroy Lane? (lot behind Southtowne Commons). Where could a green possibly be?
- ?
- Not a good use of tax money given the configuration.
- SW corner, near Sibie's subs and/or SE corner
- Behind the offices on the corner of West Pomeroy & West St. There could be a bridge (pedestrian) crossing over 116 to make a safe way for children to get to the green/park which could have swing/slide etc.
- Behind Valley transporter is a small field that would be fine.
- While it is not essential it would be nice. Perhaps off West Pomeroy or West St across from the Amherst Office Park. [2 locations also noted on map]

- around lake/pond
- In vacant lot north of W. Pomeroy Ln and south of Amherst Office Park [drawn on map - "village green" written on that lot]
- It'd be nice, but not sure where it could go. Maybe in front of Sibie's? That lot is blighted. [also drawn on map - "potential park?" in Sibie's parking lot]
- I think probably a waste of \$ unless it becomes more of a destination. We miss Curves & the massage school!
- What a good idea, but seems difficult to envision. There's field space behind the strips of malls. But it would be nice to include green areas on 116 instead of seeing only parking lots. (Just don't want it to be another rt 9!)
- (Pomeroy Village crossed out and South Amherst written in over question.)
- Area between Hess station shops & first house closest to station; preserve as much open space as possible! [drawn on map: "green space/playground/park" indicating vacant land on 116, on W Pomeroy, and across from Coach Ln on Pomeroy]
- Do not need - have too many already that we don't use
- It would be nice but there doesn't appear to be enough space.
- S.E. corner or S.W. corner/pref. S.E.
- Would be nice but have no idea where
- What are options?
- Don't know

Where should shade/street trees go?

- Both sides of West Street
- As many as possible, wherever they fit
- Don't know what "tall" or "mid-sized" means, but ones that provide shade and doesn't interfere with sight-lines. Whatever Shade Tree Committee suggests is fine
- Mix of plantings as useful (shade) and beautiful (flowering plant or colorful trees, nothing silly-petunias in pots)
- Try to beautify the area around the gas station and the big store block
- Businesses and others too close to road to make feasible. Trees would visually close off the business signs.
- Plantings are required of developments should do it
- Whatever room allows
- In empty spots nearby roads or streams
- Near the center of the village
- Between pedestrian ways & streets
- In front of Sibies
- ?
- What center & where?
- Everywhere possible
- SW corner for tall tree & smaller ornamentals
- Along Rt. 116 and W. Pomeroy Lane
- Small trees at Moan & Dove/Zhang's kitchen Plaza
- Always. Along all streets in every village center THAT is the "sign" as well as sidewalks.
- Nice. No exotics! I trust the experts on this
- In the W Pomeroy Lane green, adjacent to buildings in the village. If possible, in the parking areas
- Fruit trees would make the center homey/cozy
- By all sidewalks
- I trust the board to select what is appropriate
- Lots of trees to tone down tacky development
- Along West St & Pomeroy/W. Pomeroy, & in central green (if we acquire land)
- Not Norway Maple - they are an invasive species and hybridize w/other maples
- both sides of West St
- village center
- Again, no need unless you're going to do away with the existing trees to add more roads or sidewalks
- As they go in Downtown - alongside sidewalks
- We have plenty of trees, just don't remove what we have with elaborate sidewalks.
- South/west of the intersection of 116 & Pomeroy.
- Near bus stops.
- Mid-sized trees along Rte 116 small in green space & close to shops
- A variety of trees in a variety of places.
- Along West St.
- Along West St.
- They should plant native species where currently there are invasive species (autumn olive, honeysuckle, multiflora rose). Along 116 and Pomeroy.
- Wherever there's space
- SW & SE corner
- Along West St - both sides
- On West St in front of the pizza place
- Maybe we could close the library and buy exotic trees.

- Near benches, dispersed pattern
- Trees for shade only - set back from road, so as not to block motorists view.
- Replace the trees lost to disease on West/Pomeroy.
- Groff Park & Crocker Farm school yard look rather barren & could use some space fillers & shape.
- Along West Street
- Along town land on the strip on entire west side of West Street; at business entrances on east side, trees perceptually narrow intersection/help slow traffic
- Near sidewalk and bus stops

Where should plantings go in the village center?

- Where is the public land here - along the street?
- As useful or needed
- Beds between traffic lanes should be landscaped as they are at Amherst College
- The area is not pretty but the cost negates the aesthetics.
- Let businesses take care of plantings and maintenance.
- NONE
- Let businesses determine this and provide necessary cost(s)!!!!
- At the "central green"
- Leave to a designer
- ?
- Any
- What center & where?
- NONE
- "public areas" not indicated on map. What is a (very) "small park"? Who looks after such spaces?
- No plantings
- Encourage the property owners to do that, give them an incentive
- I can't envision this w/o more info as to location & surroundings
- Other: Cement planters; At Hess, and all other privately owned public areas
- Vegetable garden. Again, it can become a tourist destination. Public vegetable garden is unheard of.
- Near intersection benches & bus stops
- see above [I trust the board to select what is appropriate]
- Everywhere
- In "central green"
- Planter/flower boxes attached to light standards
- Wherever they can.
- There are few public areas in the confines of Pomeroy Village, it is mostly private property. Business owners have made efforts to make their property attractive by planting trees & shrubs.
- At entrances
- Near bus stops
- Around Southtowne Commons & around Hess gas station - Around a park area if one is put in.
- Along West St.
- On West St. No invasive species should be used.
- None (again, tax money)
- This depends on who (if anyone) will care for plantings! The mass of paving at the central business area needs to be broken up and softened.
- Near boundary areas
- Not sure what the public areas are: the parking lots seem to occupy a lot of space. It's pretty ugly right now, so a nice landscape design would do wonders.
- Low maintenance
- Low maintenance but manicured
- Something low maintenance & cheap. The parking lot of Sibies/Andiamo needs to be upgraded and more attractive.
- South Amherst Common; along West St
- Businesses would create. Perhaps we could have a "Tidy Town" contest as they do in Ireland, and our Village Centers could compete to be the tidiest, "prettiest" area of Town!
- None. See above. Need green (go faster light).
- Along businesses
- Depends on the location. Things will be better is Pomeroy Land Coop gets landscaped.
- Add a community garden space

Additional Comments

- My main concern is to make the intersection more pedestrian friendly and make it safer for my children to walk on Pomeroy Lane
- It would be nice to make this vital South Amherst location beautiful to look at as well as functional. Sometimes I feel as if "South" Amherst and Orchard Valley are labeled the "low income" area of Amherst and treated as such. This busy village needs fixing up and brought to represent a quaint New England town. Quality materials, good design and architecture and beautiful plantings would help.
- My greatest fear is that commercial expansion will eliminate the open space and quiet that makes living in South Amherst so enjoyable to me. I'm all for safety, attractive architecture and profitable businesses. I don't want to see over development and urban sprawl. Let's not spoil a good thing any more than we have already.
- Cars exceed speed limit at the Munson Library area. Speed bumps and perhaps flashing lights such as the ones installed at Amherst College area might help the situation.
- I think that for anything other than "safety" or "security" this is a ridiculous especially costly concept. Let the businesses cover the aesthetics. They will try to bully you into unnecessary costs. Now, if you consider bombing the business area and developing a real So. Amherst Village Center, much then could be done. Turned down by our all "stupid" Town Meeting years ago.
- Intersection is much safer now that it has traffic signals. Multiple entrances/exits around Hess station corner are a traffic hazard.
- Fill in the potholes now existing at intersection of Pomeroy & West St, as well as West Pomeroy & West St. intersection. Enforce speed limits now in place. [By survey name - "?" - It's already called "South Towne Commons"]
- We want to keep the area here as simple as possible and not spend tax dollars. Our concerns are primarily for safety, so a sidewalk for West Pomeroy and bike lane are good. Thank you for doing this survey.
- Need to work to slow traffic - Hampshire to village center and vice versa. Work for better signage for development tenants within.
- Your(s) or Town(s) opening! Statement! Opening! Statement! Says to most who have lived in South Amherst for years or more: that the town is once again listening to the minority. The local business community has done a more than adequate job so far and their investment in Pomeroy Village speaks volumes as to what can be done without town involvement!
- There needs to be more "professional" signage. The numerous sandwich boards displayed now are annoying, unsightly and probably out of code! Please investigate!
- At the present, I don't understand the "village" part of this.
- Please send the money to Amherst schools! They are witting jobs & supplies. Or maybe to LSSE programs!
- I recently moved from Western La. In Orchard Valley to S. East St but still use that intersection on a fairly regular basis.
- Consider that, in addition to the students in the area, there are people living in a low-income housing complex on Pomeroy Lane (as well as people of modest means living in the Orchard Valley area who may have trouble affording a car, especially renters in "in-law" or basement apartments), I think Pomeroy Village needs year-round bus service. The cosmetic improvements would be nice, but, to my mind, providing transportation (also including bike lanes and sidewalk lighting) to local residents who need it is more important.
- This seems all too much. We have these enormous buildings that are unable to find tenants (indeed, 2 buildings are about 80% empty). We have a library so crowded for space that books are ranged in piles on the floor. We don't have a post office. But now the town thinks we should be a "village". What does the term mean & what, exactly, do you hope to achieve? I rather imagine that the town would like to get rid of the few & useful businesses we do have so that it can build low-income/high-density housing there instead. That still won't fill the empty office buildings, but the town will feel it has struck a blow for urban planning.
- Specific proposals with reasons (whose suggestions? Why?) would be easier to evaluate. Thank you for consulting citizenry who will be affected. Also hard to respond because no mention of amount of money available for these purposes or of source of funding.
- This is a commercial area very unlike South Amherst Common. It is a main highway, not a back road. Enhance pedestrian safety & use trees & shrubs to beautify area & provide shade. Town or merchants must maintain plantings. Please make commercial signage more attractive and more uniform. Two additional, emphatic points: 1) Have minimum standards for signs that forbid (and will remove) the current excessive unattractive cluttered display of sandwich-board and other temporary signs. 2) Because most businesses at or near main intersection have pkg. lots in front, they would be "softened" with evergreen hedges along the street edge.

- My main interest would be to make crossing the street safer for pedestrians, especially to the Hess plaza, and more lighting for bus stops. I have been passed by the bus on numerous occasions during the winter because I couldn't be seen. Maybe install lights on timers such as those in some Northampton locations, so it could be lit only when necessary.
- Thank you!
- Your map, interestingly, does not define the limit of VC. It should be a compact area clearly defined by sidewalks & trees - development outside it should be stopped! Keep the village center looking like a center and keep the surrounding from looking like a center - don't let it sprawl.
- There needs to be a right turn lane onto W. Pomeroy traveling south on 116 and that intersection needs to be well-maintained. There is now a lot of traffic. There is also a potential problem with cars rushing into the intersection from Pomeroy to W. Pomeroy, from behind another car.
- Although shade trees, benches, etc, would be nice, I feel safety issues are paramount. Even with the lights, it is very hazardous for a pedestrian trying to cross West St because of traffic turning on to West St
- Entries/exits to PVC businesses should be designed for easy access/egress but signage should not be gaudy.
- First the parking lot near Hess is full of potholes which need repair.
- Need better traffic control of speed limits in area! People know if they speed in Ware that it is very likely they will get ticketed - I would like to see the same in Amherst - quality of living is affected by this, public safety and energy conservation - let us set a standard here.
- Pomeroy Village is not pedestrian friendly - traffic is fast and unyielding - more walks, more crosswalks
- Try as the town might, the area being called a town center really isn't. It feels like "town center" is being forced onto my little community. I am not at all against progress, but trying to make this area something it isn't - and doesn't want to be - is not progress. It's foolishness.
- A turn lane? From 116 to Pomeroy w/associated left turn light. The main problem is that this intersection (what is being called "the village") is not designed for pedestrians. There aren't even crosswalks! I live 1/2 mile away & would love to feel comfortable walking to and being at the "village". However, even crossing at the intersection feels dangerous! Before you go about changing - have a clear vision of the goal, e.g. to make the Village more accessible to pedestrians & those who live there as well as a destination for businesses. Integrate it into the neighborhood. Make it an attractive place to go. [drawn on map - marks Dancer Computer building with "future coffee place w/wireless internet :)]
- I was promised over a year ago that the pedestrian lines would be repainted. It still has not been done. Plus the mess near the pole has never been taken care of. It is right at the place where one crosses the street. Let's take care of older people's needs too!
- It is common knowledge that the Town of Amherst does not have the funds to carry out the context of this survey. In that sense the questions are inappropriate and somewhat hypocritical. It took over four years just to get the traffic light for the intersection in spite of hard work by the DPW. Both Plum Brook and Muddy Brook are protected riparian streams that limit expansion around the area.
- There should be a better sign indicating the entrance into Amherst Office Park from 116 south.
- Traffic moves too quickly on Pomeroy & West Pomeroy. The current speed limit of 40mph should be reduced to 30mph. Schoolchildren wait for buses here. There is a school, church, and residential houses on the street. Please change the speed limit!
- I am very pleased to see this questionnaire. I am a business and property owner at 441 West St. We need crosswalks urgently. Crossing Rt. 116 makes any pedestrian think twice. If the area is truly going to be a "village" people must want to walk from place to place. Their choices now are to dodge traffic either in the road or in the parking lots.
- We live on West Pomeroy Lane & do not want an office building to go in the lot across the street from us. A park or green in that area could be nice.
- A pedestrian light is essential for crossing West St.
- Traffic needs to be slowed along West St. & on Pomeroy. All of Pomeroy needs sidewalk - lots of people walk along this road & traffic is too fast, road too narrow.
- The town needs to reduce the speed limit in this area & needs to crack down on speeding - especially on Pomeroy Lane. I walk this road on a regular basis & have been forced to get off the shoulder of the road many times due to speeding cars. This road is very dangerous for pedestrians!!
- Enforce speeding and traffic laws better in the Town. The extra income can be used for the town.
- More police presence around "Moan and Dove" late at night (between 11:00pm and 2:00am). Too much noise pollution from car stereos - it makes my entire apartment move and wakes me up. Also too many children left unattended at laundromat across from Hess. They throw things (rocks, etc) at mailboxes, etc.
- Money would be well spent picking up trash & keeping area clean.
- [drawn on map - "encourage business to use this" written on vacant lot on West Pomeroy]
- The town is short of money. Forget this

- DRB: Thank you for the opportunity to think about/respond to this survey at this point in development of an exciting future down at this end of town! Looking forward to watching its growth into a vibrant, thriving village center.
- Encourage new businesses that will cater to neighborhood needs. Provide areas away from traffic for sitting and eating. Improve access to & use of Plum Brook and Muddy Brook. These might be locations for improved green spaces. Perhaps locate Fire station or other public/civic building on vacant space on W. Pomeroy. [also drawn on map]
- I think this is generally unnecessary! What is needed is: Better support for the existing infrastructure. The roads within Orchard Valley are an abomination! The buses continually tear them up with their starts & stops. The bus route in Orchard Valley should be eliminated and the roads should be re-surfaced. Also - minor flooding is frequent - the current drainage system is inadequate & should be upgraded & properly maintained. If we can't properly maintain what we have, why are we considering "beautifying" the area. Fix what we have first!
- I'm really happy to have the opportunity to give my opinion on this. I have always been annoyed by the term "Village Center" when it's simply a collection of office buildings, take-out restaurants, a gas station. If you really want a village, there needs to be greenery, park benches, play areas for children to invite the community in. Otherwise, it's just like every other shopping center in the U.S.
- It is pretty difficult to make this area feel village-like when a gas station is one of the anchors on the corner. The opposite corner (where Sibie's is) is fronted by a parking lot. Come to think of it the other corners have parking lots in front. This is not very conducive to creating the village feel being sought/proposed.
- Really nice survey. Well done. Also, it seems like there could be a suggested, consistent look to the business store fronts that would help the 'look' and feeling of the area. I know it's hard to mandate such things, but I feel like there's a hodgepodge of businesses, that change a lot and there's no identifiable characteristics that unite the area. Even if you said, suggested - paint the outside white and green - or something. Okay, bad idea, but something like that - and trees - window boxes - no neon - (what a radical thought). [drawn on map - "trees and sidewalk" on east side of West St from vacant lot southward to Courtyard Condos. "green (in parking lot)" indicated in front of all three corners besides the gas station. "green space" on W. Pomeroy vacant lot. "there's a nice little garden here" - indicating the south side of the Sibie's/Andiamo building. Dots for street trees drawn in between sidewalk and road on West St.]
- Thanks for asking for our input!
- The center of South Amherst includes the South Amherst Common as well as the business district. I object to a sign designating Pomeroy Village. I prefer motorists see signs saying they're "in" South Amherst. If they saw "Pomeroy Village" they might think they were elsewhere than in South Amherst. I believe such renaming of any Village should go before the voters. Why not build a casino and name it Trump Towers? Commuters in transit to jobs north of "South Amherst" should be queried. Their "straight" commute will be more convoluted and longer.
- Definitely need pedestrian crosswalk with white stripes painted clearly. The entry into Hess is dangerous being right after the lights. I know they tried to modify it but I don't think it is working. This should be carefully thought out - you don't want something like is happening at the King St/Damon Rd intersection in Noho. That would be terrible! I can't come up with a best solution but hope there is one. :) [drawn on map - "needs improvements & better usage" indicated for Sibies/Andiamo parking lot]
- Place a sign on Pomeroy near Cooperative indicating wheelchair crossing; Caution Children zone (near school & Cooperative)
- The traffic light has been a wonderful asset.
- If you widen Rt 116, it will be a speed zone.
- Getting in safe crosswalks - especially at the light - is my top priority.
- Get with the times! I have spent the last 20/30 years trying to find a bypass road to get around these kind of obstacles. I have seen the same kind of mess all over New England. Some think its pretty & quaint but I find it just wasted time on the way to work. And now when everybody has to go out of town to work - people should not be trying to revive what was just poor planning. Don't want to sit thru the debate as public figures posture for recognition & brownie points for their civic duty & or practice public speaking 101. I'm sure a report will come out in the paper. If the issue gets resolved & then finds a magic pot of funding. Thank you for the chance to be heard & vote just before we have to move. [drawn on map - 116 replaced with limited access highway that divides at Mill River for southbound side to go around Amherst Office Park and Orchard Valley, rejoining just south of Atkins. "Need to bypass all this congestion, no value to anything local, no stops allowed"]
- Given wetland/conservation land... what's the drainage/flood plan as we pave the neighborhood? I want a dry basement over any option listed here!
- We need a really nice coffee shop! A Rao's type hangout this would attract a lot of foot traffic and be good for any local business. Hampshire College is so close too!

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- ⁵ Zegeer, Charles et al. 2000. *Safety Effects of Marked Versus Unmarked Crosswalks at Uncontrolled Locations*. Transportation Research Record 1773, Paper No. 01-0505
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